



Logan Colbert
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PALMER ENGINEERING COMPANY
2817 ERICA PLACE
NASHVILLE, TN 37204
RICHARD LOGAN COLBERT, P.E. NO. 117913

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN-1
TITLE SHEET	1
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**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

SIGNATURE SHEET

Index Of Sheets
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

ROANE COUNTY

LAWNVILLE ROAD, BRIDGE OVER I-40, LM 1.70

PS&E

BRIDGE REPAIR

I-40

BRIDGE NO: 73-1224-1.70



SCALE: 1"= 1 MILE

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE PROJECT MANAGER : STEPHEN WILSON

DESIGNED BY : PALMER ENGINEERING COMPANY

DESIGNER : LOGAN COLBERT, P.E. CHECKED BY : SCOTT WILSON, P.E.

P.E. NO. 731040-M3-004

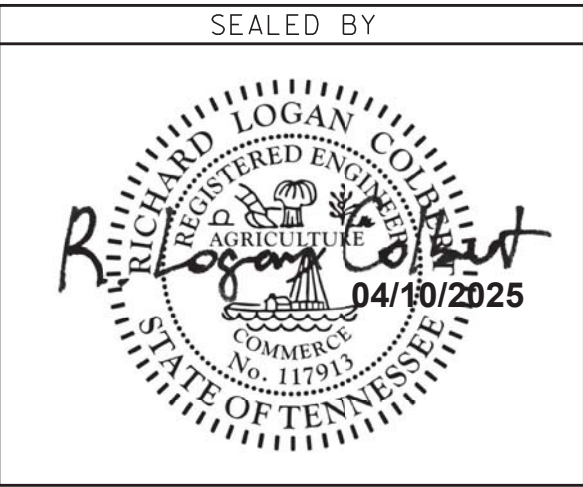
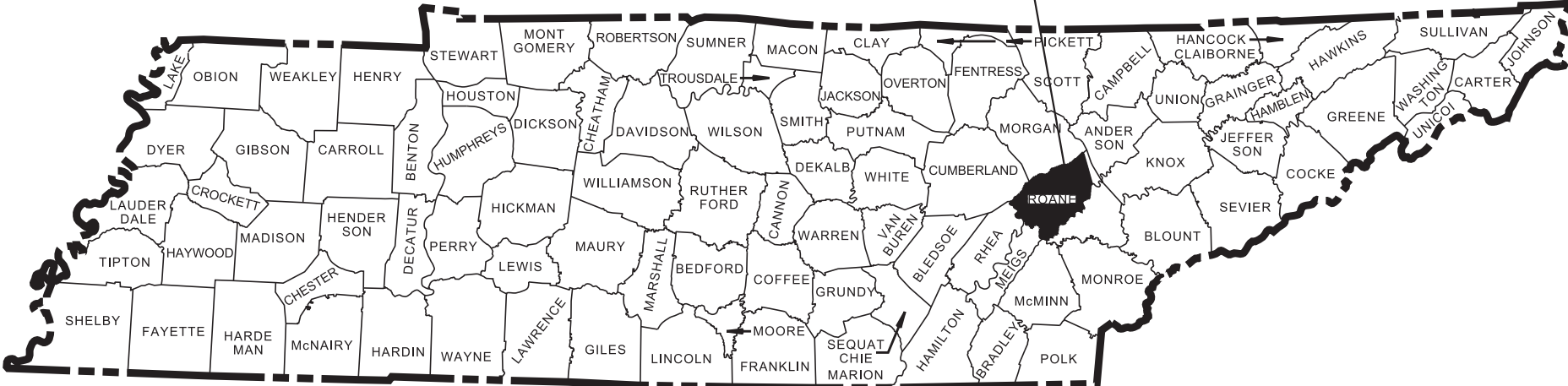
PIN NO. 112251.01

I-40		LAWNVILLE RD.	
AADT (2023)	58,668	AADT (2023)	1,494
POSTED SPEED	70 MPH	POSTED SPEED	30 MPH

TOTAL DISTURBED AREA = 0 ACRES

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	731040-M3-004	
FED. BRIDGE ID NO.	73100400019	

PROJECT LOCATION
BRIDGE ID. # 73100400019



APPROVED: WILL REID, DEPUTY COMMISSIONER/ CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, DEPUTY GOVERNOR & COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

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ROADWAY INDEX

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STANDARD DRAWINGS

DWG.	REV.	DESCRIPTION
ROADWAY DESIGN STANDARDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
SAFETY DESIGN AND GUARDRAILS		
S-CC-1	10-01-24	CRASH CUSHION
DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-S-10	04-04-12	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-19	06-12-20	STANDARD STEEL SIGN SUPPORTS
T-S-20	07-11-17	SIGN DETAILS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-16	07-30-24	LANE SHIFT FOR DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-64		WORKZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-65		LANE CLOSURE WITH LATE MERGE
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD
T-WZ-PCB1	10-10-24	10 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2	10-01-24	20 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2A	10-01-24	20 FOOT PORTABLE CONCRETE BARRIER RAIL STIFFENER TUBE

BRIDGE INDEX

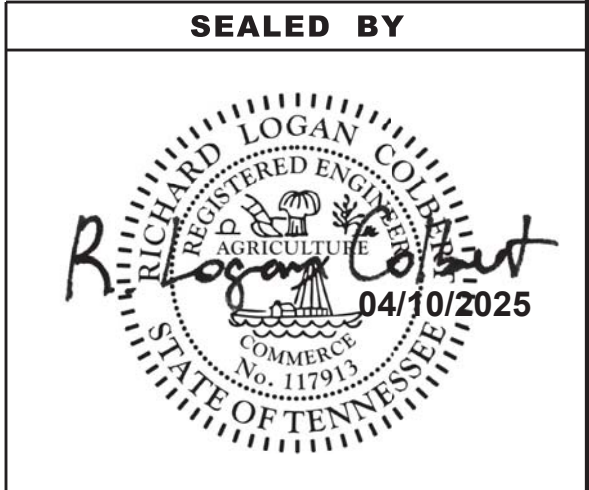
SHEET NAME	SHEET NO.	LAST REV. DATE
LAYOUT OF BRIDGE TO BE REPAIRED	BR-133-170	
ESTIMATED BRIDGE QUANTITIES AND NOTES	BR-133-171	
BEAM REPAIR DETAILS.....	BR-133-172	

LIST OF BRIDGE REFERENCE DRAWINGS
(TO BE PRINTED WITH PLANS)

F-13-113 & F-13-115

LIST OF SPECIAL PROVISIONS

SP712PTQ.....TRAFFIC QUEUE PROTECTION
SP712PQWS.....PORTABLE QUEUE WARNING SYSTEM



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION


INDEX OF SHEETS
AND
STANDARD DRAWINGS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	1B
PS&E	2025	731040-M3-004	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIROMENTAL, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 73100400019 LAWNVILLE ROAD OVER I-40 LM 1.70 (73-01224-01.70). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	ENTIRE BRIDGE

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

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BRIDGE GENERAL NOTES

SPECIFICATIONS & LOADING

- (1) **CONSTRUCTION SPECIFICATIONS:** STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), AND THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.
- (2) **DESIGN SPECIFICATIONS:** 9TH EDITION (2020) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

STEEL, CONCRETE, REINFORCING, AND FORMING

- (3) **REINFORCING STEEL:** SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.
- (4) **NOTE:** MECHANICAL BAR SPLICERS MUST BE ON THE TDOT QUALIFIED PRODUCTS LIST 27. THE BAR SPLICERS SHALL MEET AASHTO LRFD SPECIFICATIONS FOR MECHANICAL CONNECTION. WHEN EPOXY COATING IS REQUIRED, THE EXPOSED THREADS SHALL BE REPAIRED AFTER SPLICING ACCORDING TO SECTION 907 OF THE STANDARD SPECIFICATIONS. THE COST OF FURNISHING THE BAR SPLICERS, (AND EPOXY COATING WHEN REQUIRED) INCLUDING ALL LABOR AND MATERIALS NECESSARY FOR COMPLETE INSTALLATION, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REINFORCING BARS, UNLESS NOTED OTHERWISE IN PLANS.
- (5) **CONCRETE CURING:** ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

MISCELLANEOUS GENERAL NOTES

- (6) **DEMOLITION:** THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.
- (7) THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.
- (8) **REQUIREMENTS AND RESTRICTIONS FOR PHASE CONSTRUCTION**
- A. A SINGLE 11 FOOT THROUGH TRAFFIC LANE SHALL BE MAINTAINED AT ALL TIMES.

PIN NO.:	112251.01	
DESIGN BY:	R.L. COLBERT	DATE: 04/2025
DRAWN BY:	M.E. LOUCKS	DATE: 04/2025
SUPERVISED BY:	G.S. WILSON	DATE: 04/2025
CHECKED BY:	R.L. COLBERT	DATE: 04/2025

BRIDGE SPECIAL NOTES

- (9) **FORMS AND FALSEWORK:** ALL CONCRETE FORMS AND FALSEWORK SHALL BE REMOVED AFTER REPAIRS ARE COMPLETED. COST OF REMOVAL SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.
- (10) **HIGH EARLY STRENGTH CONCRETE:** THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE A MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT THE PROPOSED CONCRETE MIX WILL OBTAIN THE REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIALS AND TESTS DIVISION OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. TRAFFIC SHALL NOT BE PERMITTED BELOW ANY REPAIR AREA UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS.
- (11) THE CONTRACTOR SHALL PROVIDE 100% CONVENTIONAL FALL PROTECTION FOR WORKERS INSTALLING DECKING ABOVE 15 FEET.
- (12) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS AND CONSTRUCTION
- (13) ANY DAMAGE TO VEGETATED AREAS NOT SPECIFICALLY MENTIONED WITHIN THE PROJECT SCOPE SHALL BE PREPARED AT THE CONTRACTOR'S EXPENSE. THESE AREAS ARE TO BE RETURNED TO THEIR PRE-CONSTRUCTION STATE AND SHALL BE DETERMINED BY THE ENGINEER. ALL COSTS (LABOR AND MATERIALS) ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN ITEMS BID ON.

ESTIMATED BRIDGE QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
604-10.19	REINFORCING STEEL (REPAIRS)	LS	1
604-10.42	CONCRETE REPAIRS	C.F.	26

CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
731040-M3-004		2025	2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED BRIDGE QUANTITIES
AND NOTES
LAWNVILLE RD OVER I-40
BRIDGE NO. 731040-M3-004
FED. I.D. NO. 73100400019
ROANE COUNTY
2025

BR-133-171

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

ROAD CLOSURE

- (1) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SPECIAL NOTES

DEMOLITION

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	2A
PS&E	2025	731040-M3-004	2A

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	0.42
(1) 712-01	TRAFFIC CONTROL	LS	1
(2) 712-02.10	PORTABLE BARRIER RAIL (MASH TL-3)	L.F.	180
(2) 712-02.12	PORTABLE BARRIER RAIL, REDUCED DEFLECTION (MASH TL-3)	L.F.	110
(3) 712-02.60	TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	281
712-04.50	BARRIER RAIL DELINEATOR	EACH	6
(4) 712-06	SIGNS (CONSTRUCTION)	S.F.	308
712-08.03	ARROW BOARD (TYPE C)	EACH	1
(5) 712-08.12	QUEUE PROTECTION TRUCK	DAY	10
(6) 712-08.14	PORTABLE QUEUE WARNING SYSTEM	DAY	5
712-09.02	REMOVABLE PAVEMENT MARKING (8" BARRIER LINE)	L.F.	5400
712-09.31	REMOVABLE BLACK-OUT TAPE (8IN)	L.F.	3940
(7) 713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	3
717-01	MOBILIZATION	LS	1

FOOTNOTES

(1)	ALL COSTS ASSOCIATED WITH INSTALLING, STORING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES SHALL BE INCLUDED IN THE PRICE BID FOR EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
(2)	ITEM INCLUDES COST OF INSTALLING AND RELOCATING PORTABLE BARRIER RAIL.
(3)	THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF AASHTO MASH FOR TEST LEVEL 3. THE UNIT BID ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURER'S DRAWINGS.
(4)	ANY LOSS OR DAMAGE TO THE SIGNS SHALL BE PAID FOR BY THE CONTRACTOR.
(5)	SEE SPECIAL PROVISION SP712PTQ. QUANTITY ASSUMES 2 TRUCKS IN WESTBOUND DIRECTION.
(6)	SEE SPECIAL PROVISION SP712PQWS, STD T-WZ-65, AND SHEET 3.
(7)	CHANGEABLE MESSAGE SIGN UNITS TO BE USED FOR WIDE LOAD DETOUR SIGNAGE (SEE SHEETS T3 & T4). THESE ARE SEPARATE FROM THOSE REQUIRED FOR THE PORTABLE QUEUE WARNING SYSTEM.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY
QUANTITIES AND NOTES

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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (1)

SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2)

NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3)

INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5)

THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6)

STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7)

HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (8)

WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (10)

NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (13)

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).
- (14)

ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16)

THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (17)

ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

SUPPORT ACTIVITIES

- (18)

MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ENVIRONMENTAL

- (20)

EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)

ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5)

SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.


SCOPE OF WORK

- (6)

SEE SHEET BR-133-170 FOR PROJECT SCOPE OF WORK.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	2B
PS&E	2025	731040-M3-004	2B

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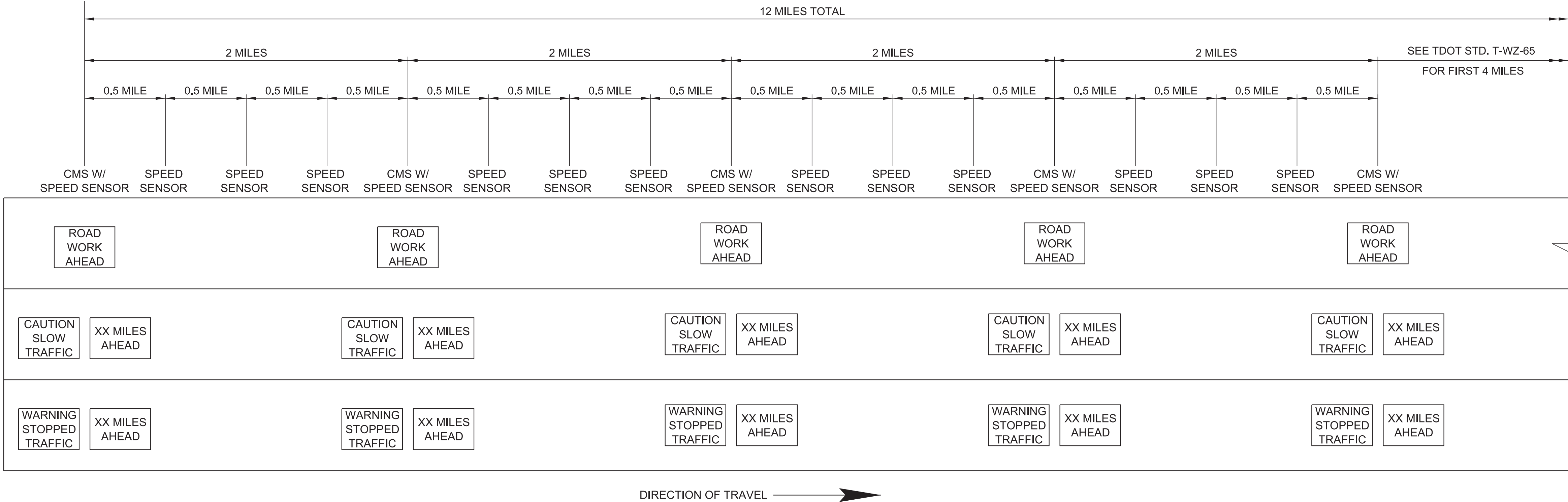


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
GENERAL AND SPECIAL
NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	73I040-M3-004	3
PS&E	2025	73I040-M3-004	3



PORTABLE QUEUE WARNING SYSTEM LAYOUT

N.T.S.
(I-40 WESTBOUND DIRECTION ONLY)

SEE TDOT SPECIAL PROVISION SP712PQWS FOR ADDITIONAL INFORMATION.

IF TRAFFIC BACKS UP BEYOND THE LAST CHANGEABLE MESSAGE SIGN UNIT, TDOT PERSONNEL SHOULD BE NOTIFIED SO ADJUSTMENTS CAN BE MADE TO RELOCATE THE LAST CMS WITH SPEED SENSOR TO THE NEW END OF THE QUEUE AND ADJUST SPACING OF THE INTERMITTENT SPEED SENSORS.

PQWS CHANGEABLE MESSAGE SIGN UNITS ARE TO BE PROGRAMED WITH "ZIPPER" MERGE SYSTEM MESSAGES. MESSAGES AND LOCATIONS ARE TO BE DETERMINED BY THE ENGINEER. ANY COSTS ASSOCIATED WITH INTEGRATING ADDITIONAL "ZIPPER" MERGE MESSAGES OR DEVICES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 712-08.14, PORTABLE QUEUE WARNING SYSTEM, DAY.

IF THE PQWS IS AT ANY POINT INOPERABLE WHILE A LANE CLOSURE IS IN EFFECT, THE CONTRACTOR SHALL IMMEDIATELY ALERT THE DISTRICT ENGINEER. ADDITIONALLY, THE CONTRACTOR SHALL DISPATCH PTQ TRUCKS IN ACCORDANCE WITH TDOT SPECIAL PROVISION SP712PTQ WITHIN 4 HOURS OF THE SYSTEM BEING INOPERABLE AND SHALL REMAIN ON-SITE UNTIL THE SYSTEM IS AGAIN FULLY FUNCTIONING. THE COST OF THESE VEHICLES WILL BE INCIDENTAL TO THE PQWS PAY ITEM.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PORTABLE QUEUE
WARNING SYSTEM
LAYOUT

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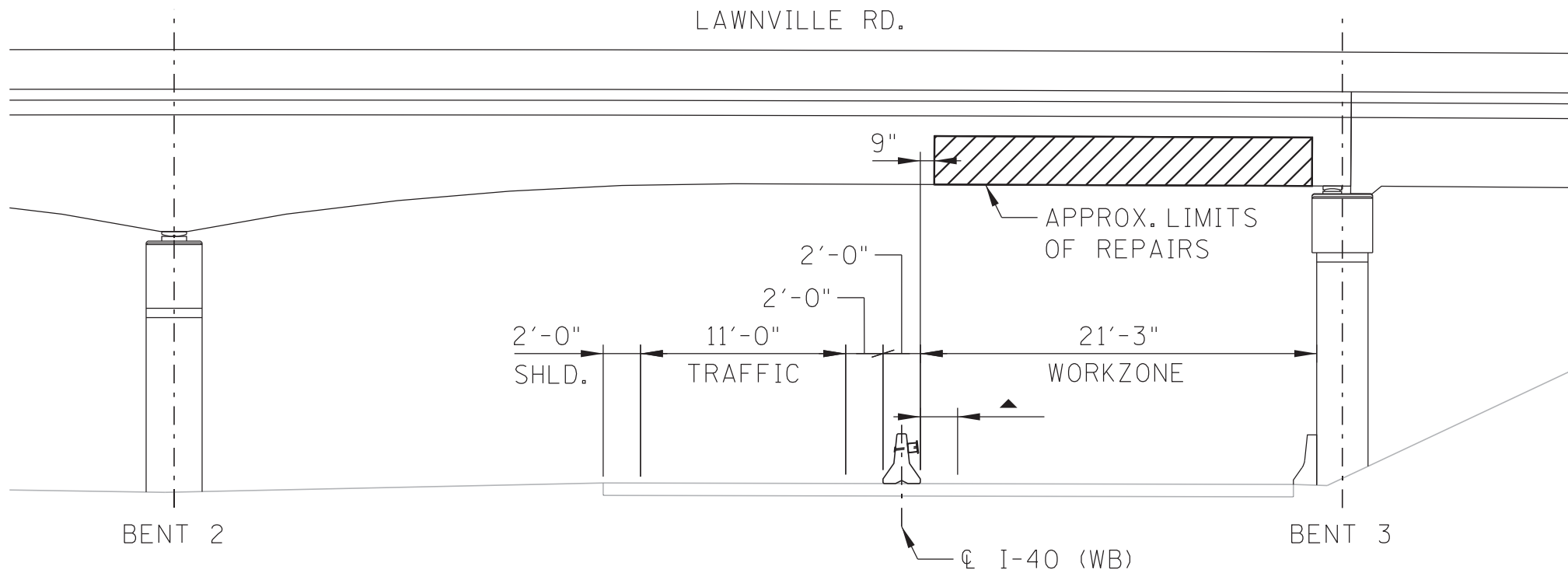
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	73I040-M3-004	T1
PS&E	2025	73I040-M3-004	T1

TRAFFIC CONTROL SIGN QUANTITIES							
TYPE	DESCRIPTION	SIZE (IN INCHES)			S.F.	No.	SIGNS (CONST.) 712-06, S.F.
		L	x	W			
E5-1	EXIT	48	x	36	12	1	12
E5-2	EXIT OPEN	48	x	36	12	1	12
G20-2	END ROAD WORK	48	x	24	8	4	32
M1-1	INTERSTATE ROUTE SIGN	36	x	36	9	2	18
M3-4	CARDINAL DIRECTION (WEST)	36	x	18	4.5	2	9
M4-8	DETOUR	36	x	15	3.75	2	7.5
M6-3	DIRECTIONAL ARROW (STRAIGHT)	30	x	21	4.375	2	8.75
W1-4L	REVERSE CURVE (LEFT)	48	x	48	16	2	32
W1-4R	REVERSE CURVE (RIGHT)	48	x	48	16	2	32
W20-1	ROAD WORK 1 MILE	48	x	48	16	2	32
W20-1	ROAD WORK AHEAD	48	x	48	16	1	16
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48	x	48	16	2	32
W20-5R	RIGHT LANE CLOSED 1500 FT	48	x	48	16	2	32
W4-2R	LANE ENDS (RIGHT)	48	x	48	16	2	32
TOTAL							307.25

TRAFFIC CONTROL SPECIAL NOTES

- THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
- CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
- THE CONTRACTOR IS REQUIRED TO PROVIDE LANE SHIFTS WHERE NECESSARY TO ROUTE TRAFFIC AROUND CONSTRUCTION.
- NO TRAFFIC SHALL BE DETOURED OR ROADWAY CLOSED, ABANDONED, OR REMOVED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
- ADVANCE WARNING SIGNS ARE TO BE PLACED PRIOR TO BEGINNING OF CONSTRUCTION AND REMAIN IN PLACE UNTIL THE COMPLETION OF THIS PROJECT.
- PERMANENT SIGNS AND PERMANENT MARKINGS SHALL BE IN PLACE BEFORE COMPLETED ROADWAYS ARE OPEN TO TRAFFIC.
- SEE THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" FOR TRAFFIC DETAILS NOT SHOWN, GENERAL TRAFFIC CONTROL NOTES, AND SIGN DETAILS.
- CONTRACTOR TO COVER ALL CONFLICTING SIGNS DURING CONSTRUCTION. COVERINGS SHALL BE REMOVED AT COMPLETION OF CONSTRUCTION. COST ASSOCIATED WITH COVERING AND UNCOVERING SIGNS TO BE INCLUDED IN ITEM 712-06, SIGNS (CONSTRUCTION).
- THE CONTRACTOR IS TO COORDINATE ALL LANE CLOSURES WITH THE TDOT REGIONAL OFFICE AND THE COUNTY ENGINEER.
- MINIMUM TRAVEL LANE WIDTH TO BE 11 FEET (STRIPED).
- CONFLICTING RUMBLE STRIPS SHALL BE REMOVED PRIOR TO SHIFTING TRAFFIC FOR CONSTRUCTION, AND THEN REPLACED BACK IN-KIND AFTER CONSTRUCTION IS COMPLETE. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN THE COST BID FOR ITEM 411-12.01.
- ALL PORTABLE CONCRETE BARRIERS (PCB) DEFLECT DURING AN IMPACT. CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING IMMEDIATELY BEHIND THE PCB.

A PORTABLE QUEUE WARNING SYSTEM SHALL BE UTILIZED ALONG I-40 WB. SEE TDOT STANDARD DRAWING T-WZ-65, SHEET 3, AND TDOT SPECIAL PROVISION SP712PQWS FOR ALL PERTINENT INFORMATION.



I-40 WB TYPICAL SECTION
(PERPENDICULAR TO I-40)

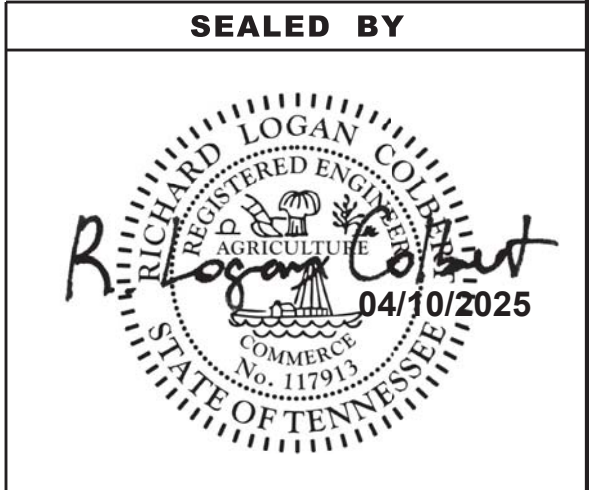
▲ DENOTES: 2'-0" DYNAMIC DEFLECTION ZONE



LIMITS OF REPAIRS ASSUMED BASED ON TDOT INSPECTION REPORT DATED 4/17/2024. CONTRACTOR SHALL FIELD VERIFY THE REPAIR LOCATIONS AND ADJUST THE SHOULDERS OF THE TRAVELWAY AS NEEDED AND AS DIRECTED BY THE ENGINEER.



CHANGEABLE MESSAGE SIGN UNITS ARE TO BE USED FOR THE WIDE LOAD DETOUR MESSAGES. THE WIDTH OF THE MAXIMUM WIDE LOAD IN THE MESSAGE SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
PLAN AND NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	T2
PS&E	2025	731040-M3-004	T2

DETOUR

WEST

INTERSTATE
40

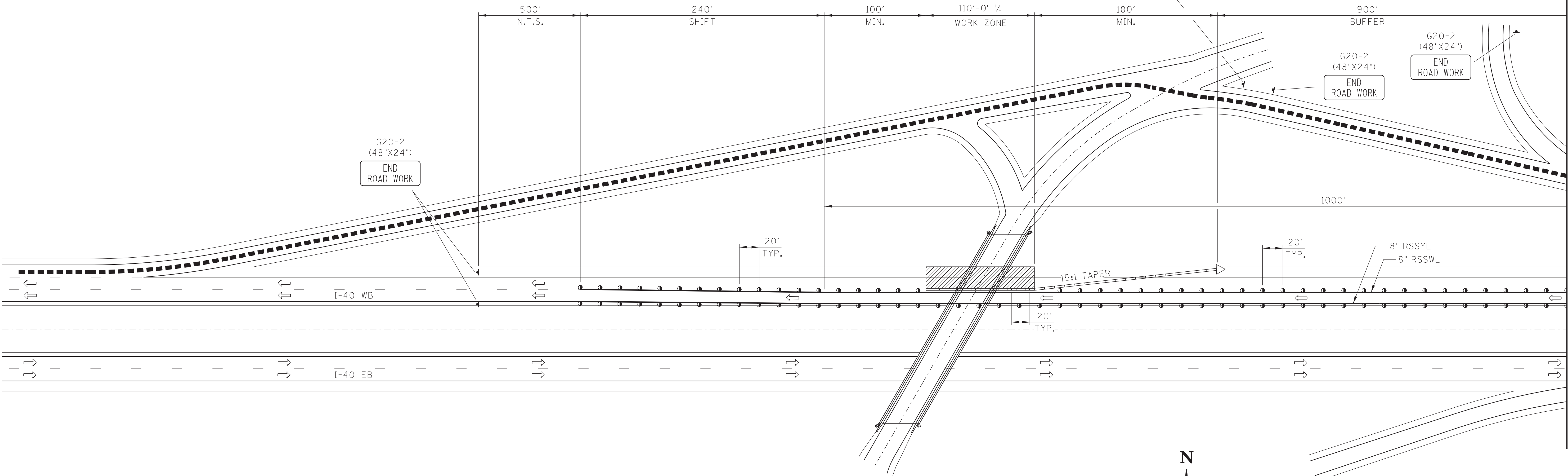
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M4-8

M3-4

M1-1

M6-3



TRAFFIC CONTROL LEGEND			
SYMBOL	ITEM	SYMBOL	ITEM
●	FLEXIBLE DRUMS	...	FLASHING YELLOW ARROW BOARD
▨	WORK ZONE	⇨	FLOW OF TRAFFIC
▬▬▬▬	PORTABLE BARRIER RAIL (WITH DELINEATORS)	N.T.S.	NOT TO SCALE
◁	CRASH CUSHION	RSSYL	REMOVABLE SINGLE SOLID YELLOW LINE
▮	SIGN	▬▬▬▬	WIDE LOAD DETOUR ROUTE
RSSWL	REMOVABLE SINGLE SOLID WHITE LINE	┤	CHANGEABLE MESSAGE SIGN UNIT (CMS)

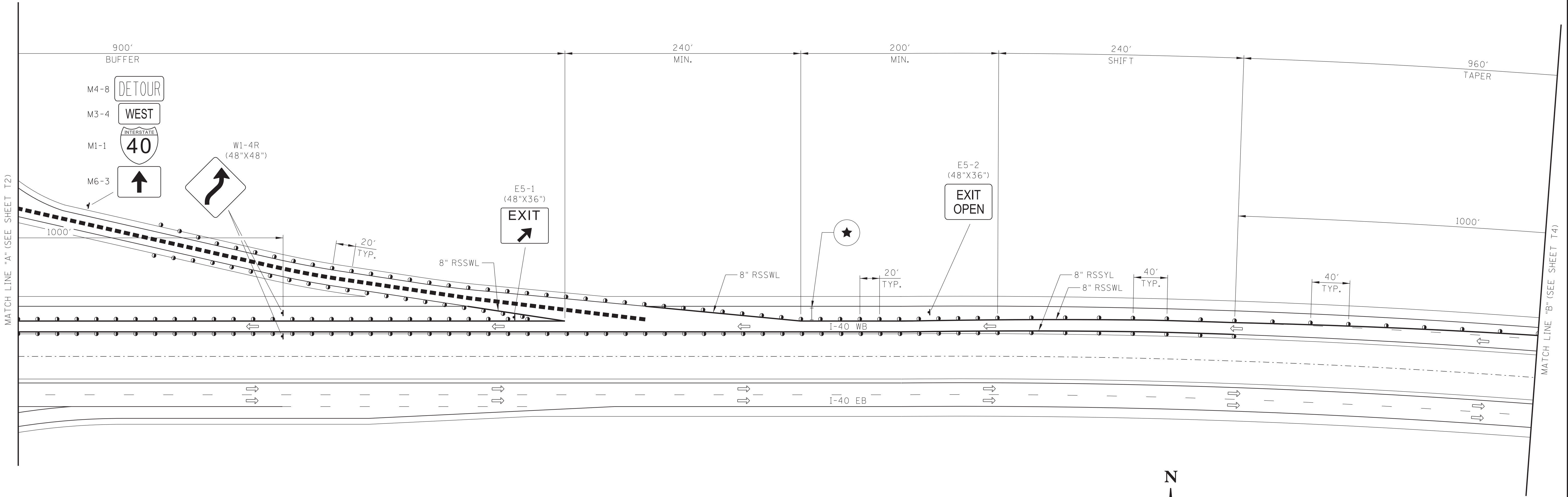
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
PLAN AND NOTES

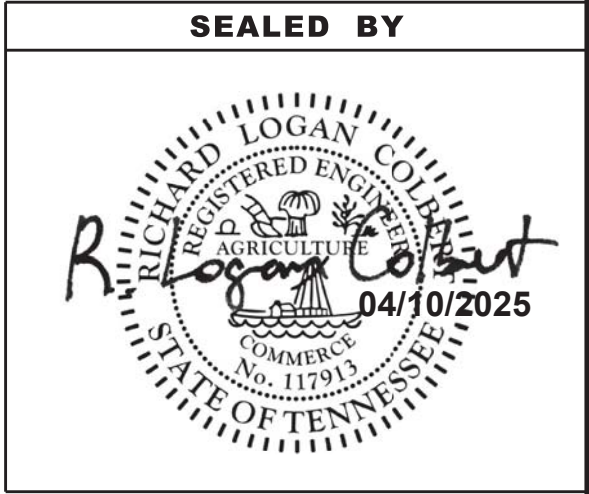
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	T3
PS&E	2025	731040-M3-004	T3



TRAFFIC CONTROL LEGEND			
SYMBOL	ITEM	SYMBOL	ITEM
●	FLEXIBLE DRUMS	...	FLASHING YELLOW ARROW BOARD
▨	WORK ZONE	⇌	FLOW OF TRAFFIC
▬▬▬▬	PORTABLE BARRIER RAIL (WITH DELINEATORS)	N.T.S.	NOT TO SCALE
◁	CRASH CUSHION	RSSYL	REMOVABLE SINGLE SOLID YELLOW LINE
▮	SIGN	▬▬▬▬	WIDE LOAD DETOUR ROUTE
RSSWL	REMOVABLE SINGLE SOLID WHITE LINE	— —	CHANGEABLE MESSAGE SIGN UNIT (CMS)

★ CMS FOR WIDE LOAD DETOUR. MESSAGE TO BE DETERMINED BY THE ENGINEER. (ITEM NO. 713-16.01)

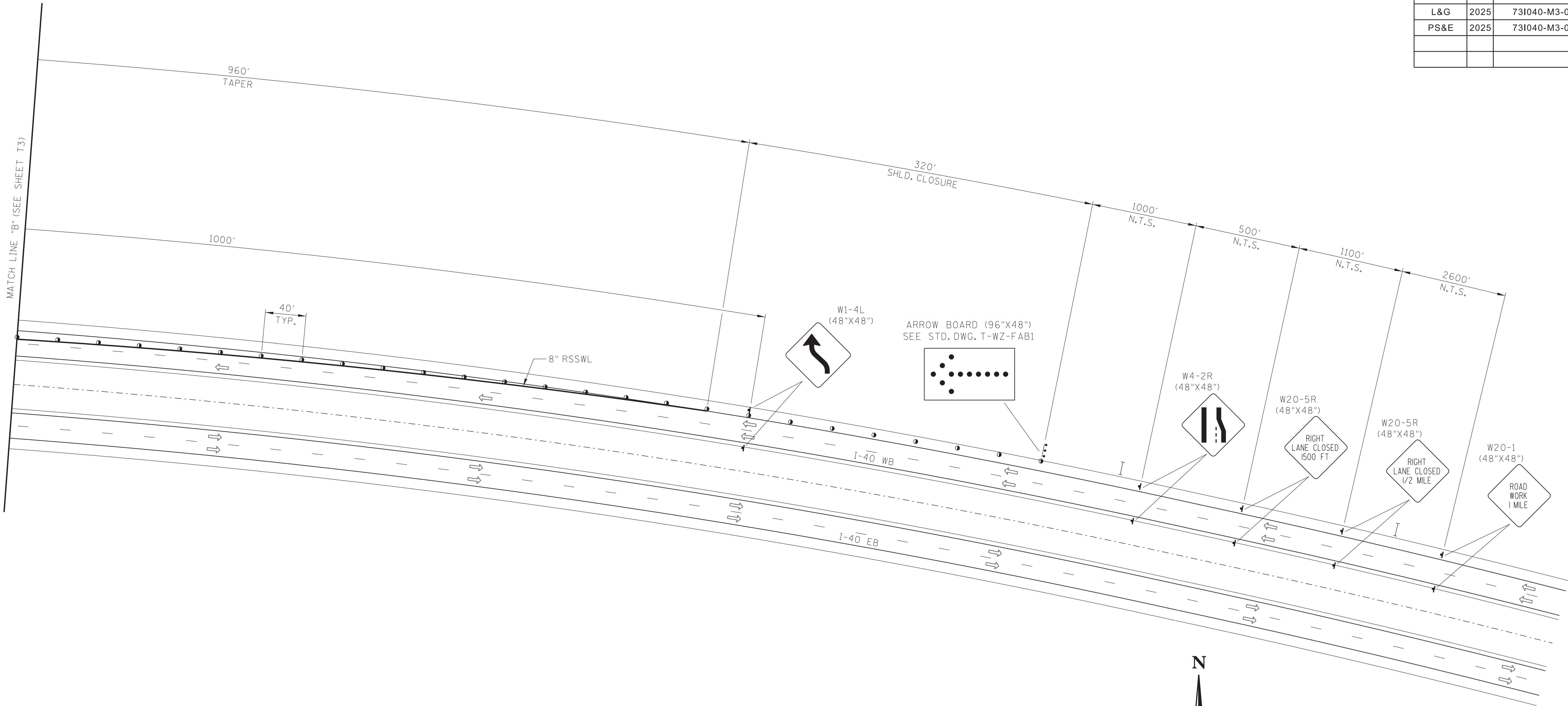


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

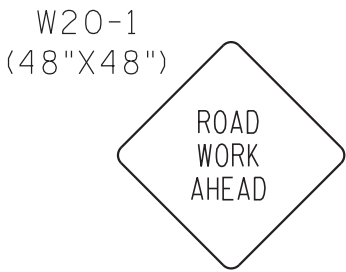
TRAFFIC CONTROL
PLAN AND NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	T4
PS&E	2025	731040-M3-004	T4



TRAFFIC CONTROL LEGEND			
SYMBOL	ITEM	SYMBOL	ITEM
	FLEXIBLE DRUMS		FLASHING YELLOW ARROW BOARD
	WORK ZONE		FLOW OF TRAFFIC
	PORTABLE BARRIER RAIL (WITH DELINEATORS)	N.T.S.	NOT TO SCALE
	CRASH CUSHION	RSSYL	REMOVABLE SINGLE SOLID YELLOW LINE
	SIGN		WIDE LOAD DETOUR ROUTE
RSSWL	REMOVABLE SINGLE SOLID WHITE LINE		CHANGEABLE MESSAGE SIGN UNIT (CMS)

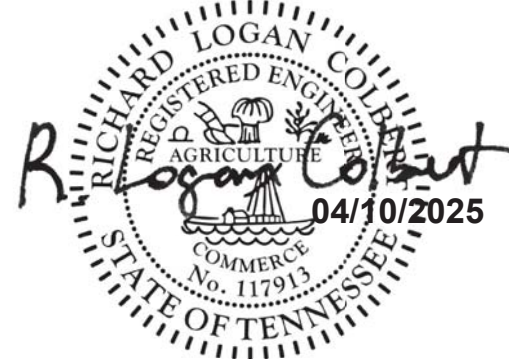


ONE (1) ROAD WORK AHEAD SIGN TO BE LOCATED AT EXIT 356 I-40 WEST ENTRANCE RAMP.

I-40 WEST CLOSED TO WIDE LOADS OVER XX FT.
FOLLOW DETOUR AT EXIT 355 LAWNVILLE RD.

CHANGEABLE MESSAGE SIGNS FOR WIDE LOAD DETOUR TO BE LOCATED AT 1/2 MILE AND 1 MILE BEFORE EXIT 355 I-40 WEST EXIT RAMP. MESSAGE MAY BE MODIFIED AS DIRECTED BY THE ENGINEER. (ITEM NO. 713-16.01)

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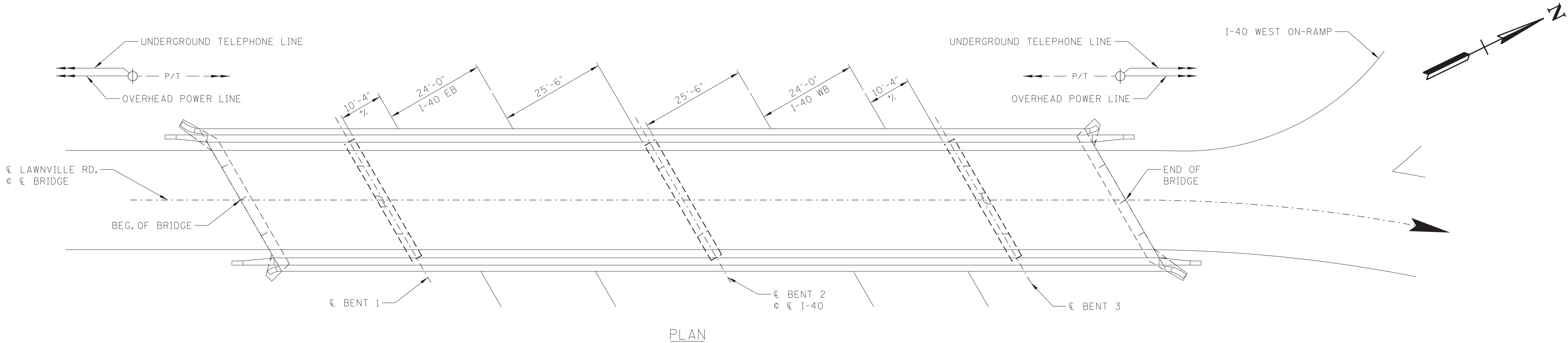
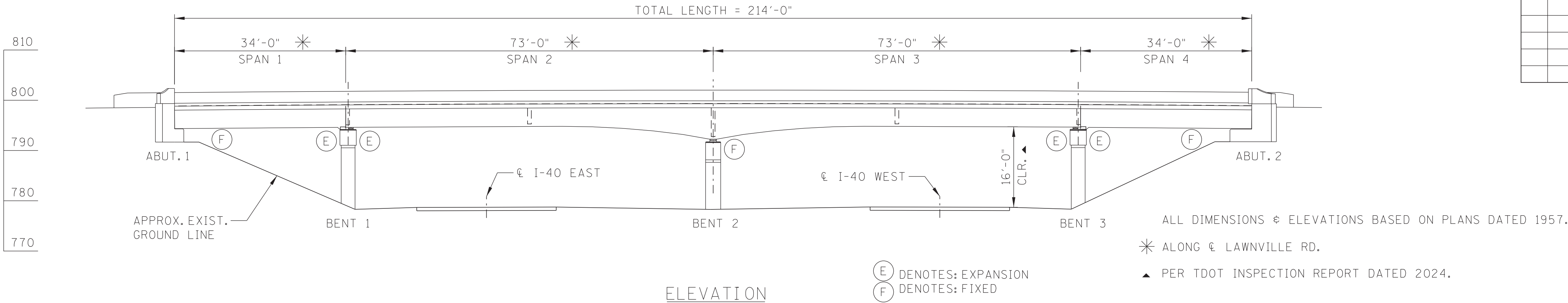


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
PLAN AND NOTES

CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
731040-M3-004		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



SCOPE OF WORK

1. PROVIDE REQUIRED TRAFFIC MAINTAINING ONE LANE OF TRAFFIC FOR I-40 WB (BELOW).
2. REPAIR DAMAGED CONCRETE BEAMS (BEAM "B" AND BEAM "D" IN SPAN 3) OVER I-40 WB.

DRAWING
LAYOUT OF BRIDGE TO BE REPAIRED
ESTIMATED BRIDGE QUANTITIES AND NOTES
BEAM REPAIR DETAILS

LIST OF DRAWINGS

DRAWING NO.
BR-133-170
BR-133-171
BR-133-172

LAST REV. DATE

INTERSTATE 40
POSTED SPEED LIMIT = 70 M.P.H.
ADTT = 58,668

LAWNVILLE RD.
POSTED SPEED LIMIT = 30 M.P.H.
ADTT = 1,494

LIST OF BRIDGE REFERENCE DRAWINGS
(TO BE PRINTED WITH PLANS)

F-13-113 & F-13-115

LIST OF SPECIAL PROVISIONS

TDOT SPECIAL PROVISIONS
SP712PTQ & SP712PQWS

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE TO BE REPAIRED
LAWNVILLE RD OVER I-40
BRIDGE NO. 731040-M3-004
FED. I.D. NO. 73100400019
ROANE COUNTY
2025


BR-133-170

Palmer
ENGINEERING

PIN NO.: 112251.01

DESIGN BY: R.L. COLBERT DATE: 04/2025
DRAWN BY: M.E. LOUCKS DATE: 04/2025
SUPERVISED BY: G.S. WILSON DATE: 04/2025
CHECKED BY: R.L. COLBERT DATE: 04/2025

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PIN NO.: 112251.01

DESIGN BY: R.L. COLBERT

DRAWN BY: M.E. LOUCKS

SUPERVISED BY: G.S. WILSON

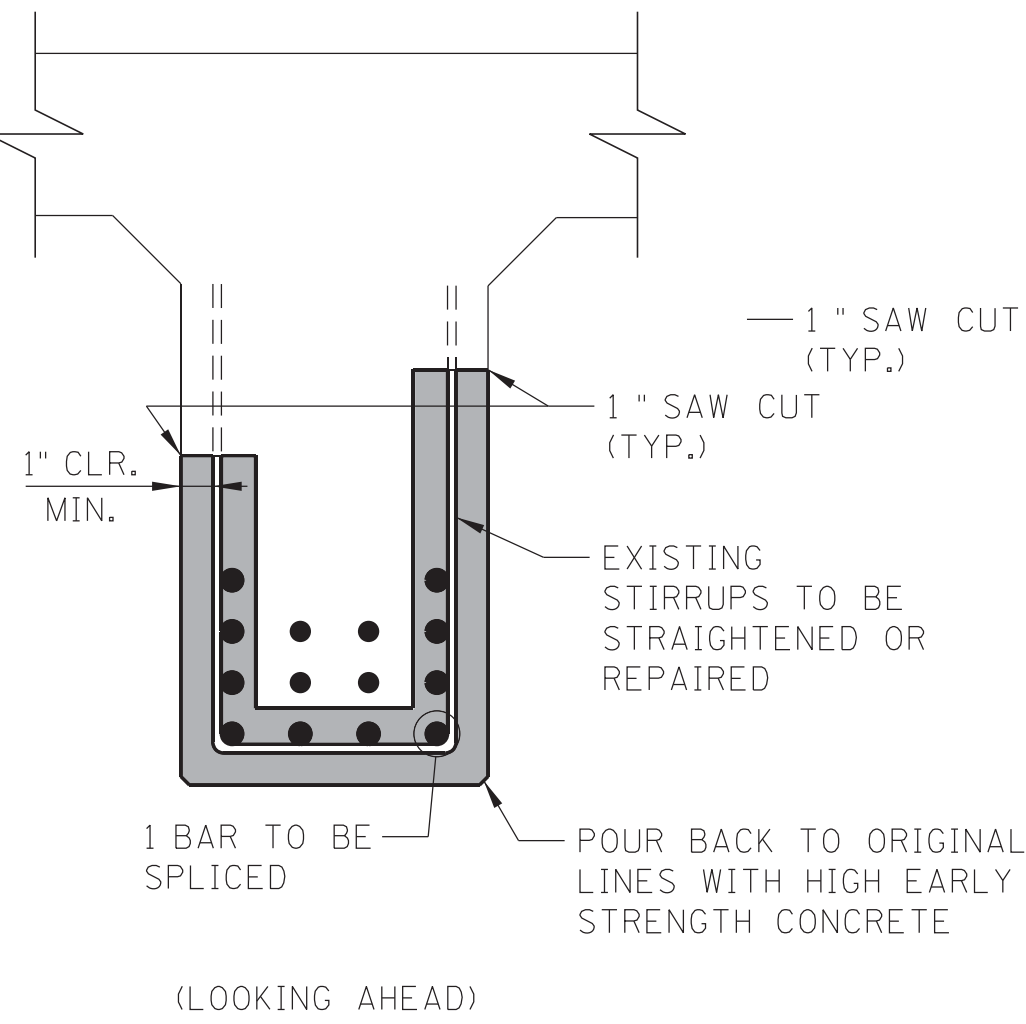
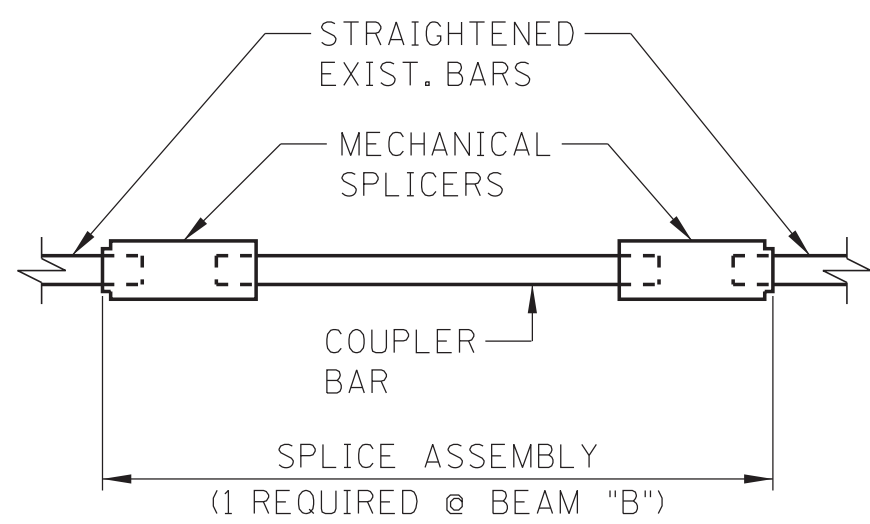
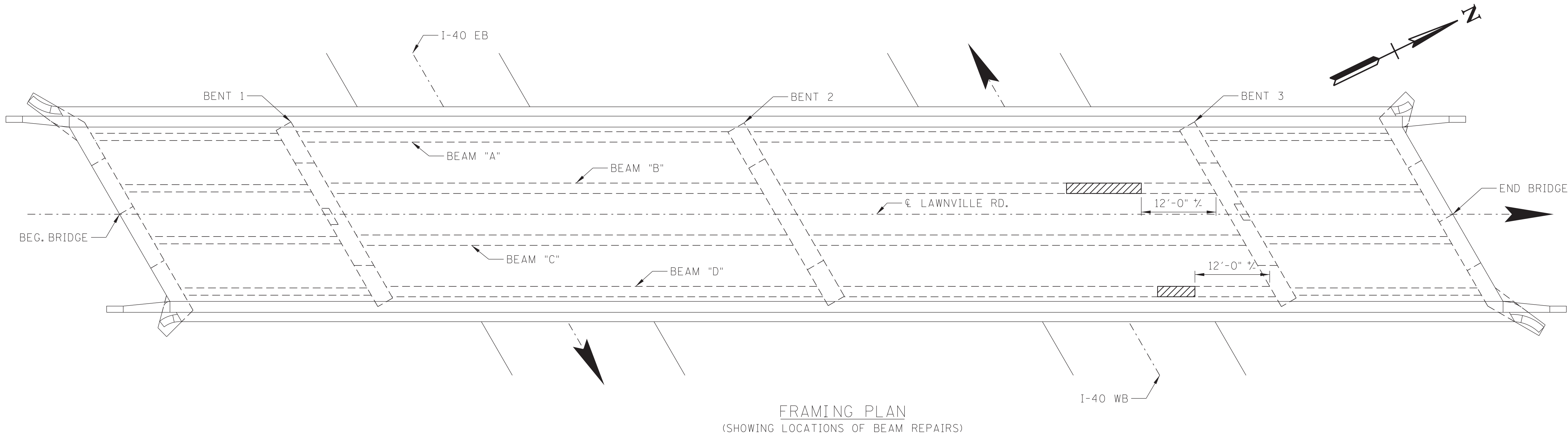
CHECKED BY: R.L. COLBERT

DATE: 04/2025

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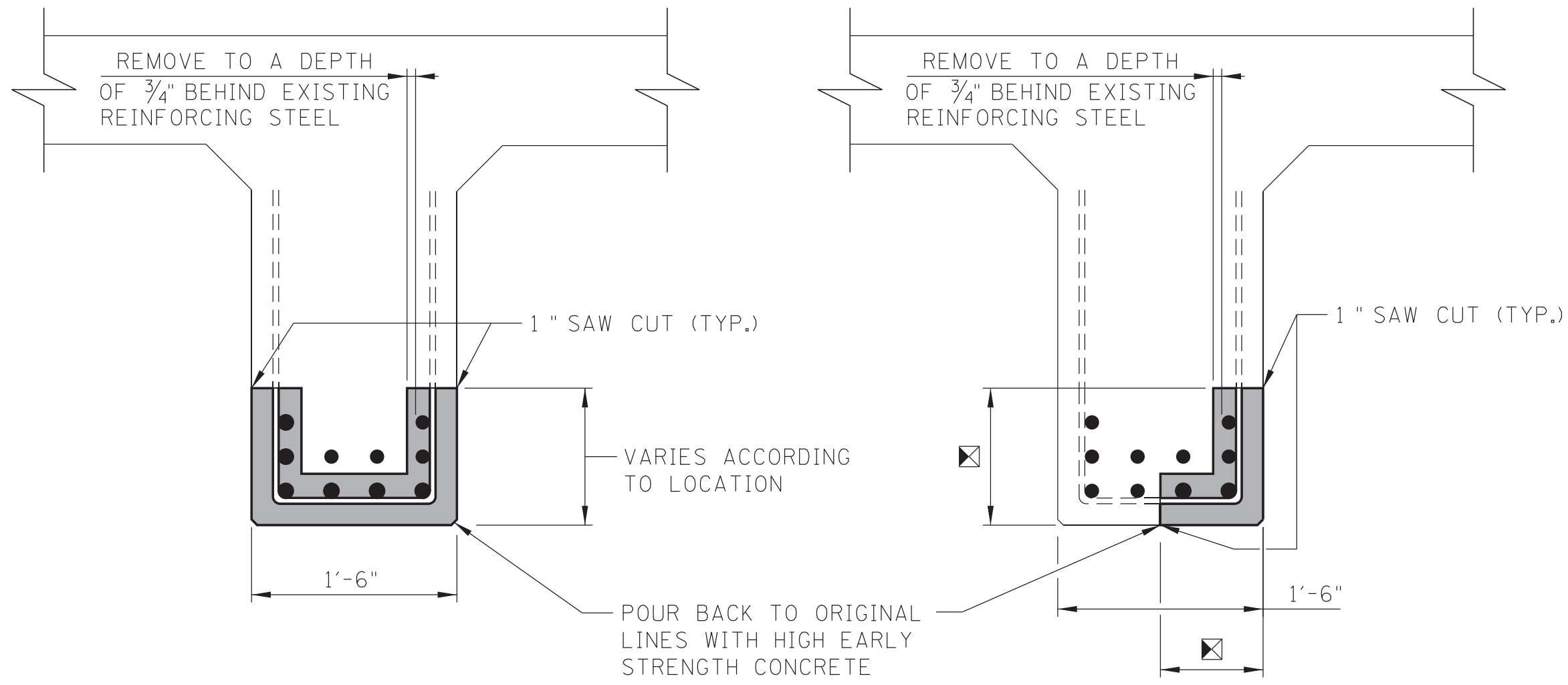
REINFORCING STEEL REPAIR DETAILS

DAMAGED/BROKEN LONGITUDINAL STEEL IS TO BE STRAIGHTENED, CUT, AND REPAIRED WITH MECHANICAL BAR SPLICERS AND A COUPLER BAR AS SHOWN ABOVE. ANY BROKEN OR DAMAGED STIRRUPS SHALL BE REPAIRED WITH NEW #5 BARS, COUPLED TO THE EXISTING STIRRUPS AND FIELD BENT TO MATCH THE EXISTING BEAM CROSS SECTION.

BAR SPLICE ASSEMBLIES SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS, TDOT SPECIFICATIONS, AND THE DETAILS ON THIS SHEET. MECHANICAL BAR SPLICERS MUST BE INCLUDED ON THE TDOT QUALIFIED PRODUCTS LIST 27.

ROUND #11 BARS ARE ASSUMED TO BE THE REINFORCING STEEL DAMAGED BY COLLISION. REINFORCING STEEL AND MECHANICAL BAR SPlicer SIZE ARE ASSUMED BASED ON DESIGN PLANS COMPARED WITH INSPECTION REPORT PHOTOS OF THE COLLISION DAMAGE. THE CONTRACTOR SHALL FIELD VERIFY EXISTING REINFORCING STEEL.

THE COST OF ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO REPAIR DAMAGED/BROKEN REINFORCING STEEL SHALL BE INCLUDED UNDER ITEM NO. 604-10.19, REINFORCING STEEL (REPAIRS), L.S.



BEAM REPAIR DETAILS

ANY DEFORMED EXISTING REINFORCING STEEL TO BE STRAIGHTENED BACK TO ORIGINAL DESIGN CONFIGURATION.

THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, STRAIGHTENING AND COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.

CONTRACTOR TO TAKE EXTREME CARE WHEN REMOVING DELAMINATED CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL. ALL EXISTING REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS TO THE BEAMS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR. COST TO BE INCLUDED IN ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.

ESTIMATED QUANTITIES	
LOCATION OF REPAIR	ITEM 604-10.42 APPROX. REPAIR AREAS (C.F.)
BEAM "B"	21
BEAM "D"	5
TOTAL	26

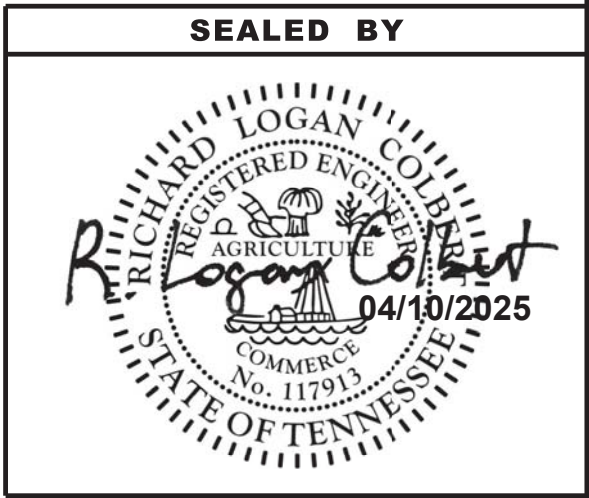
CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
731040-M3-004		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BEAM REPAIR DETAILS
LAWNVILLE RD OVER I-40
BRIDGE NO. 731040-M3-004
FED. I.D. NO. 73100400019
ROANE COUNTY
2025

BR-133-172



4/10/2025 8:20:50 PM J:\Structures\12477-28_Roane_Co_Lawnville_Road_over_I-40\Drawings\Final_dgn\12247-28_RD_UTILITY NOTES.dgn

UTILITY NOTES

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

ELECTRIC:

ROCKWOOD ELECTRIC UTILITY

P.O. BOX 108

341 W ROCKWOOD STREET

ROCKWOOD, TN 37854

CONTACT: CONNER NELSON

OFFICE PHONE: 865-717-5416

EMAIL: CNELSON@ROCKWOODELECTRIC.COM

TELEPHONE:

AT&T

9733 PARKSIDE DRIVE

KNOXVILLE, TN 37922

CONTACT: JAY FRAZIER

OFFICE PHONE: 865-387-2685

EMAIL: JF092G@ATT.COM

CABLE:

COMCAST

1725 WEARS VALLEY ROAD

SEVIERVILLE, TN 37862

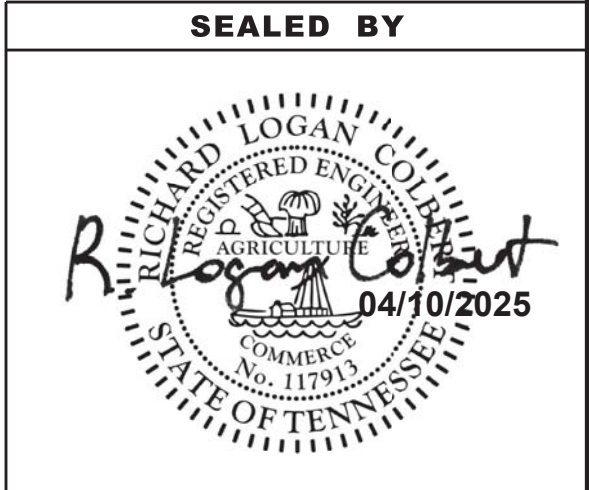
CONTACT: JASON KELLY

OFFICE PHONE: 865-862-5061

CELL PHONE: 862-437-0295

EMAIL: JASON_KELLY@COMCAST.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	731040-M3-004	U1-1
PS&E	2025	731040-M3-004	U1-1

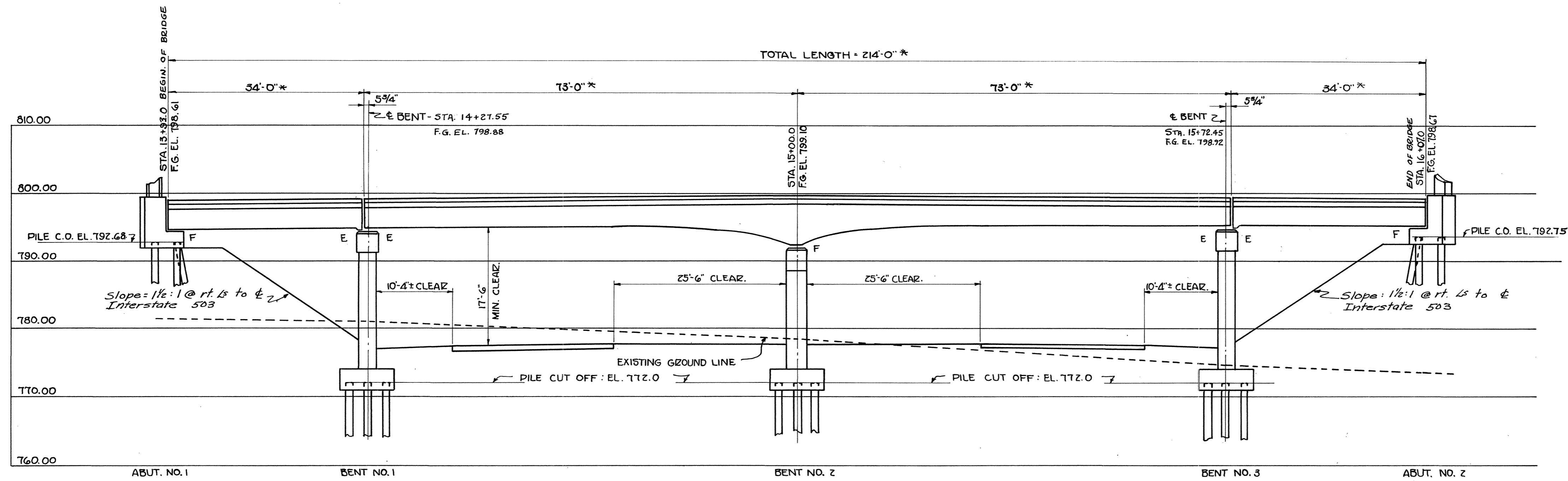


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND UTILITY OWNERS

I-503(1)

NOTE: E = EXPANSION
F = FIXED



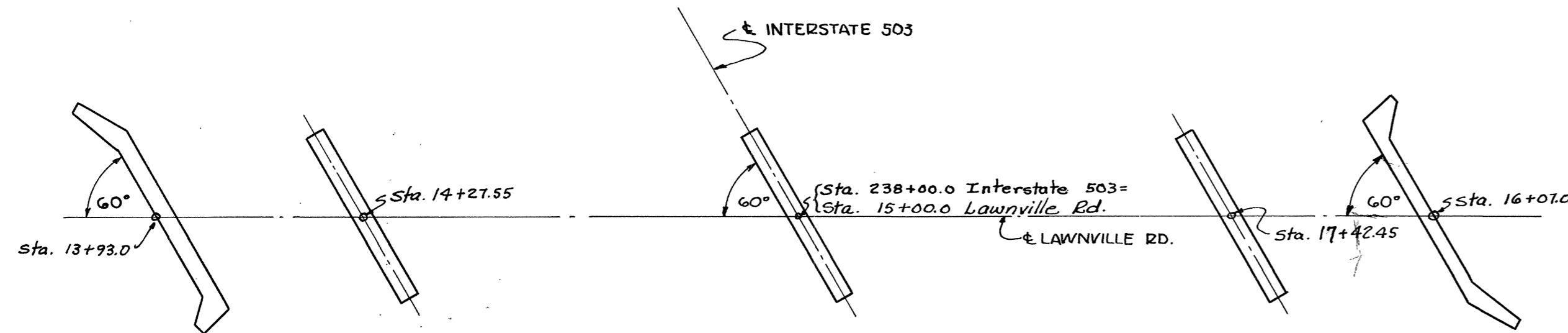
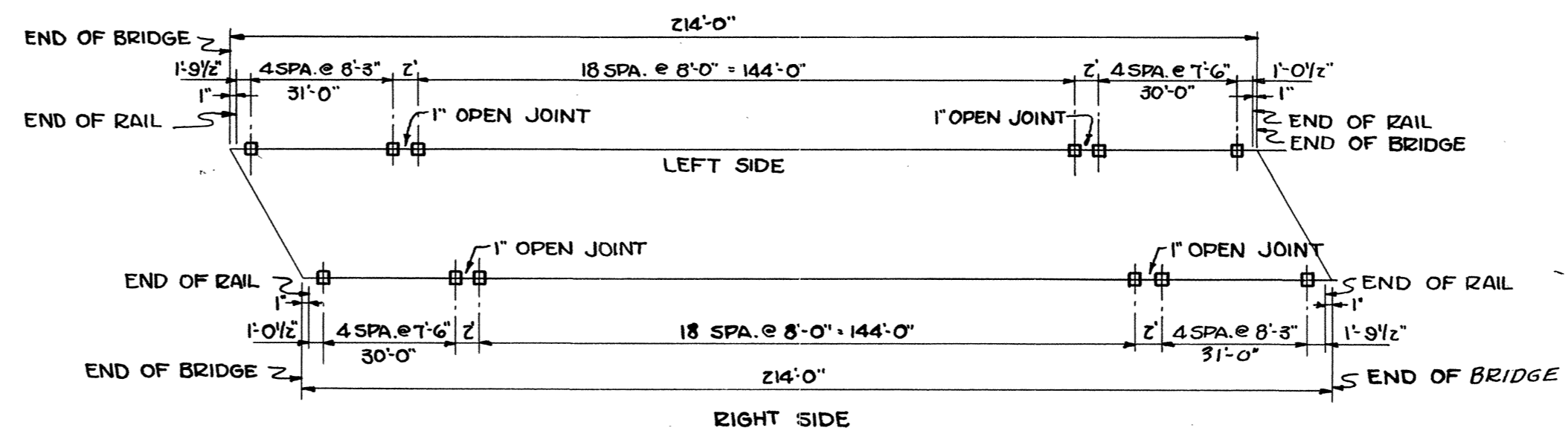
ELEVATION - Rt. 15 To Interstate 503

SCALE 1"=10'
*MEASURED ALONG INTERSTATE 503

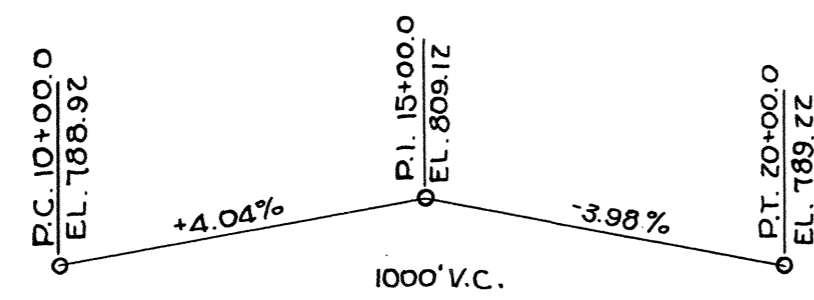
NOTE: FOUNDATIONS FOR BENTS SHALL BE EXCAVATED TO THE PILE CUT OFF ELEVATION SHOWN; ROD SOUNDINGS SHALL THEN BE MADE AS DIRECTED BY THE ENGINEER. FROM THE RESULTS OBTAINED THE BRIDGE ENGINEER WILL DECIDE IF PILES WILL BE USED, OR THE FOOTINGS CARRIED TO ROCK. THE COST OF ROD SOUNDINGS SHALL BE INCLUDED IN THE COST OF ITEMS BID ON. ALL PILES SHALL BE DRIVEN TO ROCK OR MINIMUM BEARING OF 35 TONS. FOR ABUTMENTS 1 & 2, AND 40 TONS FOR BENTS 1-3.

HANDRAIL NOTE:

BUILD HANDRAIL ACCORDING TO STANDARD F-10-47 WITH FOLLOWING POST SPACINGS:



PLAN



SKETCH SHOWING GRADES

Note: Elevations shown are based on Fin.Gr.

ESTIMATED QUANTITIES

ITEM	EXCAVATION CU. YDS.		CONCRETE CLASS "A" CU. YDS.	STEEL - LBS.		ALUMINUM HANDRAIL LIN. FT.	ROCK DRILL LIN. FT.	STEEL PILES 10" DIA. 42" X 4" LIN. FT.	STEEL PILES 12" DIA. 42" X 4" LIN. FT.
	DRY	ROCK		REINFORCING	STRUCTURAL				
SUPERSTRUCTURE	-	-	372.4	101247	5947	428	-	-	-
ABUTMENT NO. 1	-	-	17.9	1483	-	-	-	246	-
ABUTMENT NO. 2	-	-	17.9	1483	-	-	-	246	-
BENT NO. 1	55	-	30.6	3910	-	-	-	-	75
BENT NO. 2	82	-	38.0	6248	-	-	-	-	171
BENT NO. 3	55	-	30.6	3910	-	-	-	-	100
TOTALS	192	-	507.4	118281	5947	428	-	542	346

Note: Cost of all joint material to be included in the unit price bid for Class "A" Concrete

: Includes all Bearing Plates

** NO ALTERNATE PILES PERMITTED.

LEVEL BEARING NOTE:

At all bearings, beams shall be constructed, and Bearing Plates set in such a manner that these plates will be level. For typical sketch see Dwg. F-13-102

GENERAL NOTES

SPECIFICATIONS: STANDARD ROAD & BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS.
LOADING: H20-S16-44
CONCRETE: TO BE CLASS "A"
REINFORCING STEEL: SEE SPECIFICATIONS
FORMS & FINISH: SEE SPECIFICATIONS
ROCK FOUNDATIONS: SEE SPECIFICATIONS
PILES: IN ABUTMENTS 1 & 2 - 10" DIA. 42" IN BENTS 1-3 - 12" DIA. 42" X 4"

SELF LUBRICATING BEARING PLATES: SEE SPECIAL PROVISION

LIST OF DRAWINGS

DWG. No.

HANDRAIL
SEE NOTE THIS SHEET
SUPERSTRUCTURE
ABUTMENT NOS. 1 & 2
BENT NOS. 1, 2 & 3
BENT NO. 2
PILES

F-10-47
F-13-114, F-13-115, & F-13-116
F-13-117
F-13-118
F-13-119
F-2-118 & F-13-105

28' ROADWAY WITH SAFETY CURBS.

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
AND PUBLIC WORKS
NASHVILLE

LAYOUT OF BRIDGE

LAWNVILLE ROAD INTERCHANGE

STA. 13+93.0

ROANE COUNTY

CORRECTED: Fred Guse
APPROVED: Herbert M. Staley
STATE HIGHWAY ENGINEER

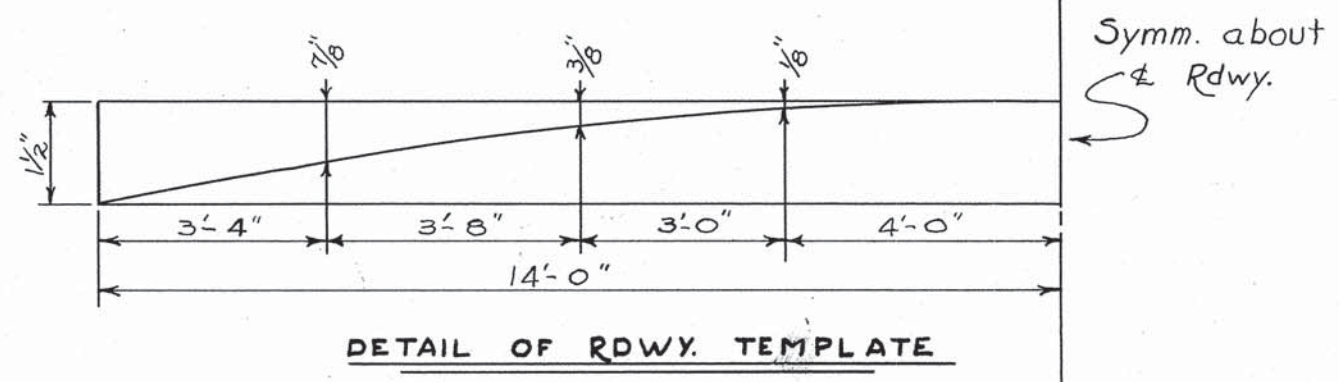
F-13-113

DESIGNED BY: B.H.
DRAWN BY: A.BURKE
CHECKED BY: A.BURKE
DATE: 4-24-57

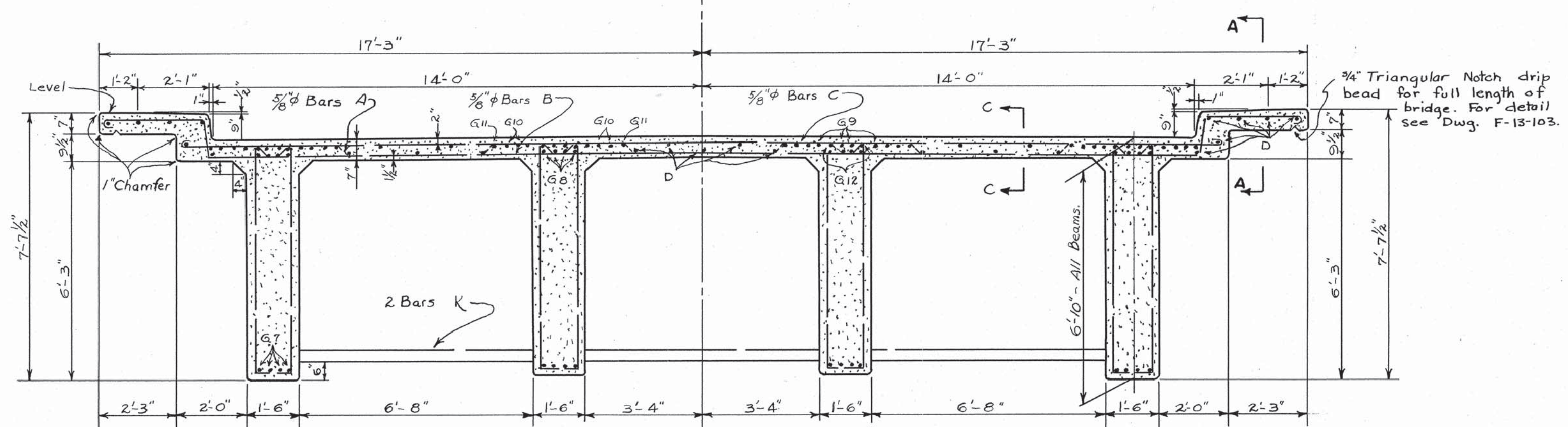
SOUNDING SKETCH

G = GROUND EL.
R = ROCK EL.

I-50(7)

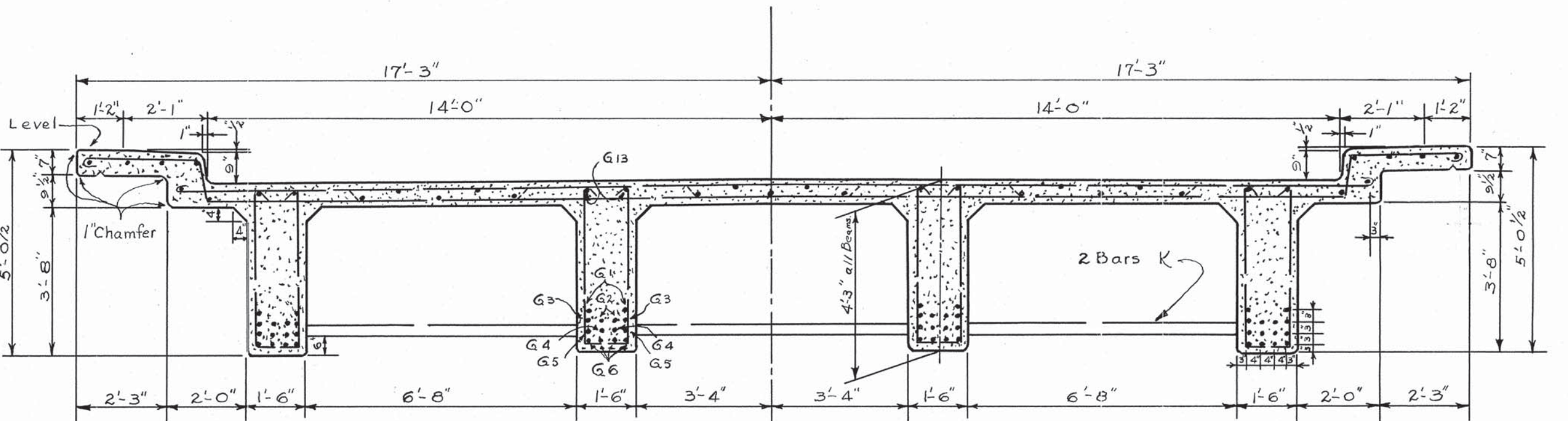


DETAIL OF RDWY. TEMPLATE

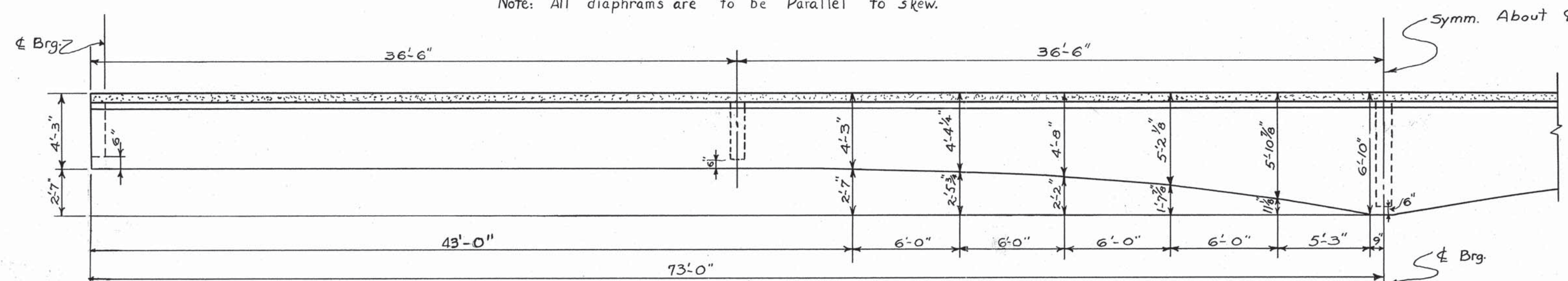


TYPICAL SECTION @ FIXED SUPPORT

Note: All diagrams are to be Parallel to skew.

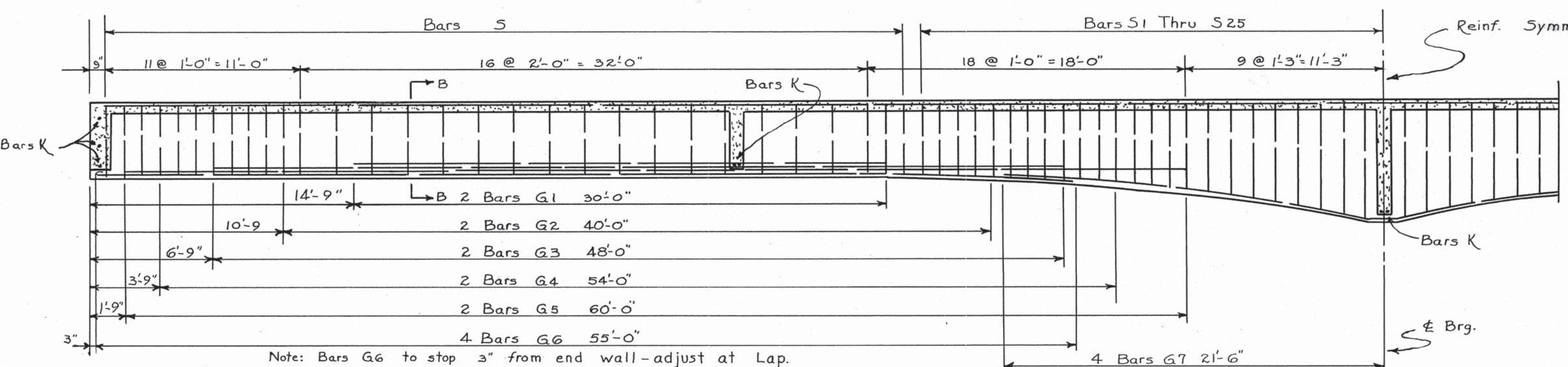


SECTION B-B



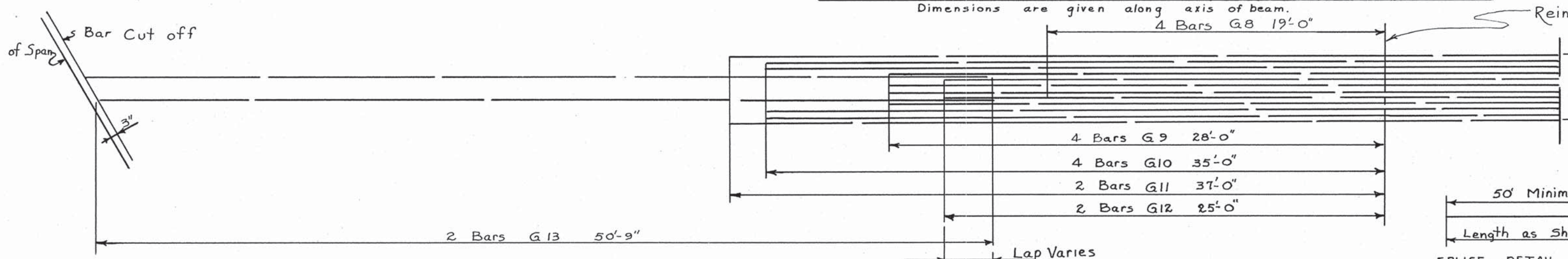
SECTIONAL ELEVATION A-A SHOWING GIRDER ORDINATES

Dimensions are given along axis of Beam.



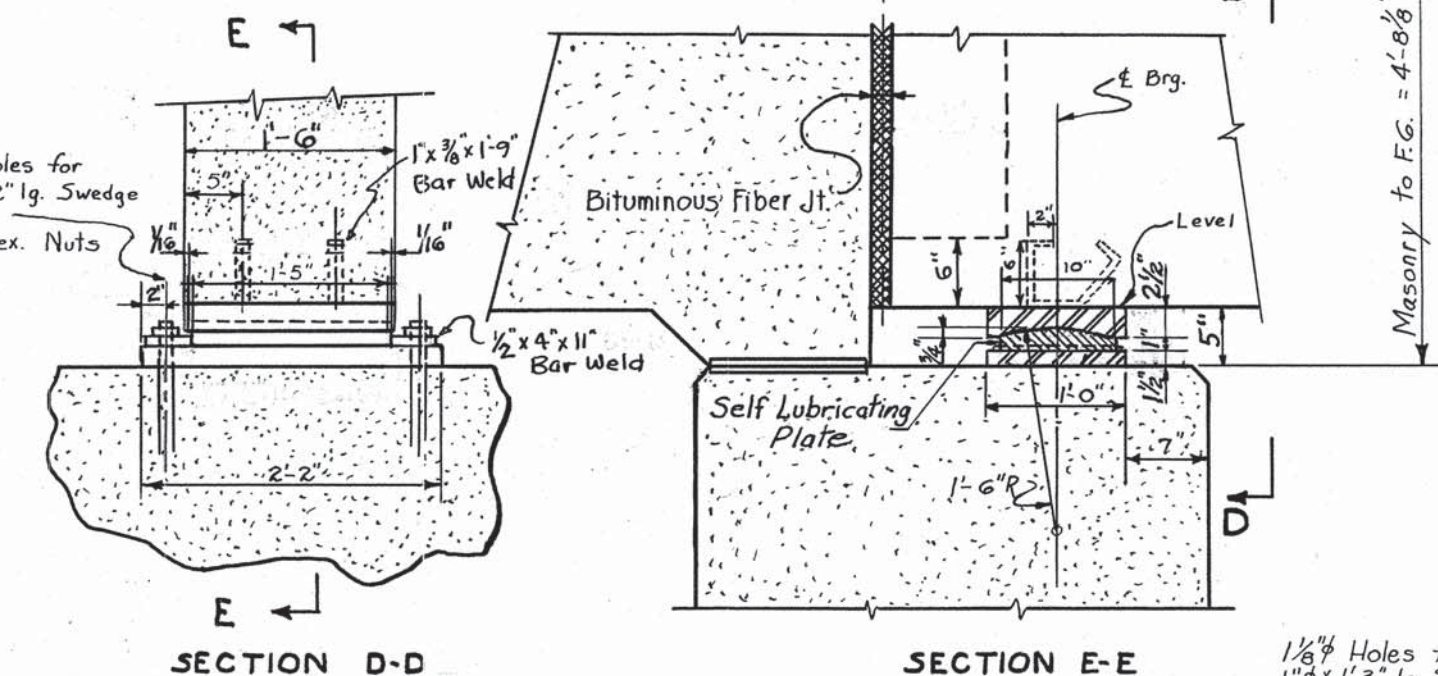
SECTION C-C SHOWING STIRRUPS AND POSITIVE MOMENT REINFORCEMENT

Dimensions are given along axis of beam.



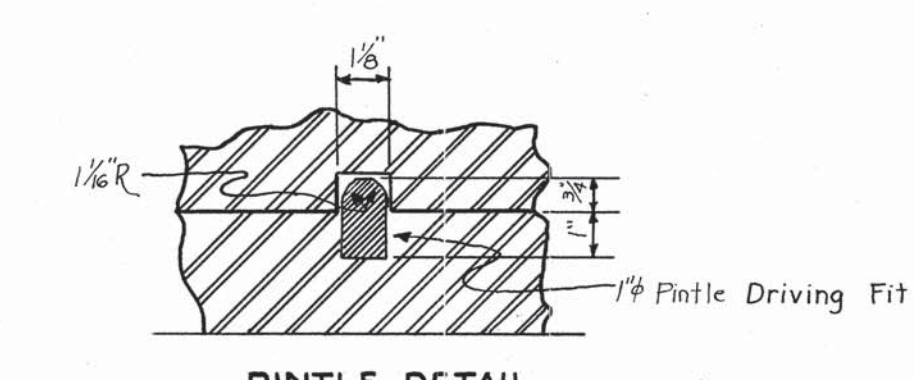
ARRANGEMENT OF NEGATIVE MOMENT REINFORCEMENT

SPICE DETAIL FOR BARS G10&G11
NOTE: SPICES TO BE MADE ON ALTERNATE SIDES OF SUPPORT.

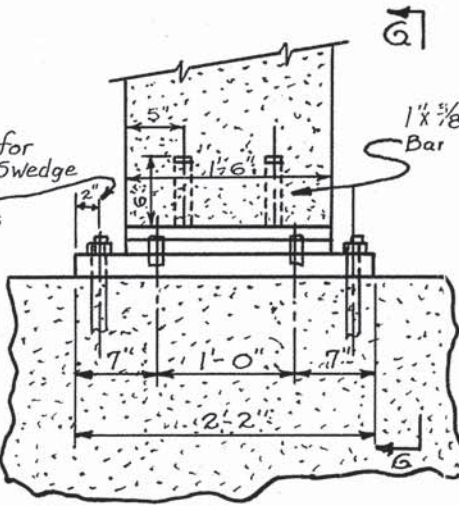


SECTION D-D

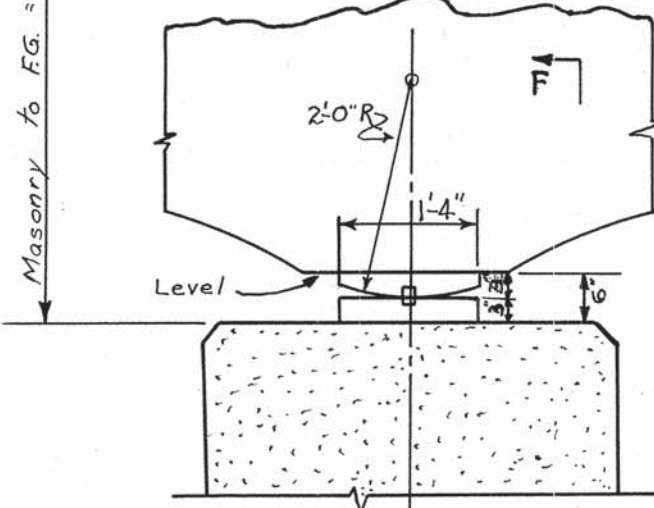
SECTION E-E



PINTLE DETAIL



SECTION F-F



SECTION G-G

EXPANSION DETAIL @ BENT NO. 1

Note: Same for Bent No. 3

8 Required

FIXED DETAIL @ BENT NO. 2

4 Required

BILL OF STEEL FOR GIRDERS SPANS 2&3

Bar No.	Size	Dimension	Length	Bar No.	Size	Dimension	Length
G1	16	1/4"	30'-0"	G7	16	1/4"	44'-6"
G2	16	"	40'-0"	G8	16	"	38'-0"
G3	16	"	48'-0"	G9	16	"	56'-0"
G4	16	"	54'-0"	G10	16	"	73'-0"
G5	16	"	60'-0"	G11	8	"	77'-0"
G6	32	"	56'-0"	G12	8	"	50'-0"
G7	16	"	44'-6"	G13	16	1/4"	50'-9"
G8	16	"	38'-0"	K	12	3/8"	29'-6"
G9	16	"	56'-0"	S	240	"	10'-0"
G10	16	"	73'-0"	S1	8	"	4'-0"
G11	8	"	77'-0"	S2	8	"	4'-0"
G12	8	"	50'-0"	S3	8	"	4'-0"
G13	16	1/4"	50'-9"	S4	8	"	4'-1"
K	12	3/8"	29'-6"	S5	8	"	4'-1"
S	240	"	10'-0"	S6	8	3/8"	4'-2"
S1	8	"	4'-0"				
S2	8	"	4'-0"				
S3	8	"	4'-0"				
S4	8	"	4'-1"				
S5	8	"	4'-1"				
S6	8	3/8"	4'-2"				

Note: See Dwg. F-13-116 for Slab Bill of Steel

ESTIMATED QUANTITIES	
SPANS 2&3	
CLASS "A" CONCRETE	2610 CU. YDS.
REINFORCING STEEL	80141 LBS.
STRUCTURAL STEEL	5771 LBS.

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
AND PUBLIC WORKS
NASHVILLE
SUPERSTRUCTURE FOR
SPANS 2&3
LAWNVILLE RD. INTERCHANGE
ROANE CO.
1957

CORRECT: *Red Guss*
APPROVED: *Charles M. Pater*
STATE HIGHWAY ENGINEER

DESIGNED BY: H.W.D.
DRAWN BY: B.H.
TRACED BY: G. Mullican
CHECKED BY: _____
DATE: 3-20-57
DATE: 3-25-57
DATE: 4-22-57
DATE: _____

Bars S Thru. S25

Bars G6

Bars G7

TEAC CND. 4/24/57 Add